

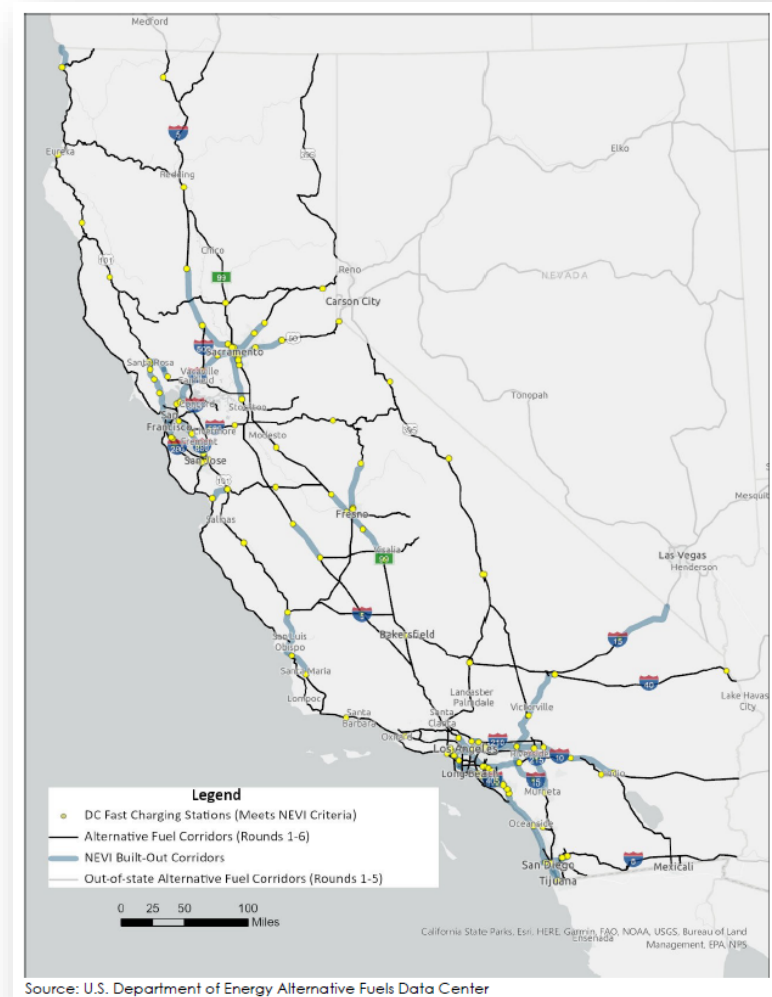


# California's Draft National Electrical Vehicle Infrastructure (NEVI) Grant Program

September 26, 2022

# Program Background Information

- Federal funds administered by Caltrans and CEC
- California to receive \$384 million over 5-years
- California's NEVI Plan approved on September 14, 2022.
- Goal: deployment of Direct Current Fast Chargers (DCFC) for light duty vehicles along California's Alternative Fuel Corridors (AFC).
- Status: federal and state guidelines are currently under development
- California is accepting comments through September 28, 2022.



# Project & Equipment Requirements

## Federal

- Public stations available 24/7/365
- Stations  $\leq 50$  miles apart and  $\leq 1$  mile from corridor/off-ramp
- 50% of chargers must be in disadvantaged communities (DAC)/low-income communities (LIC)
- 40% of chargers must be benefit Justice40 communities
- Site power  $\geq 600$  kW supporting  $\geq 150$  kW per port and across four ports simultaneously
- At least four 150 kW networked chargers per station
  - CCS connectors
  - OCPP 2.0.1
  - ISO-15118 hardware ready
  - Nationally Recognized Testing Lab certification

## State

- Site must have a restroom
- Dispensers must be capable of  $\geq 350$  A
- All conduits runs sized for 350 kW to each dispenser
- At least 1 stub-out for future installation

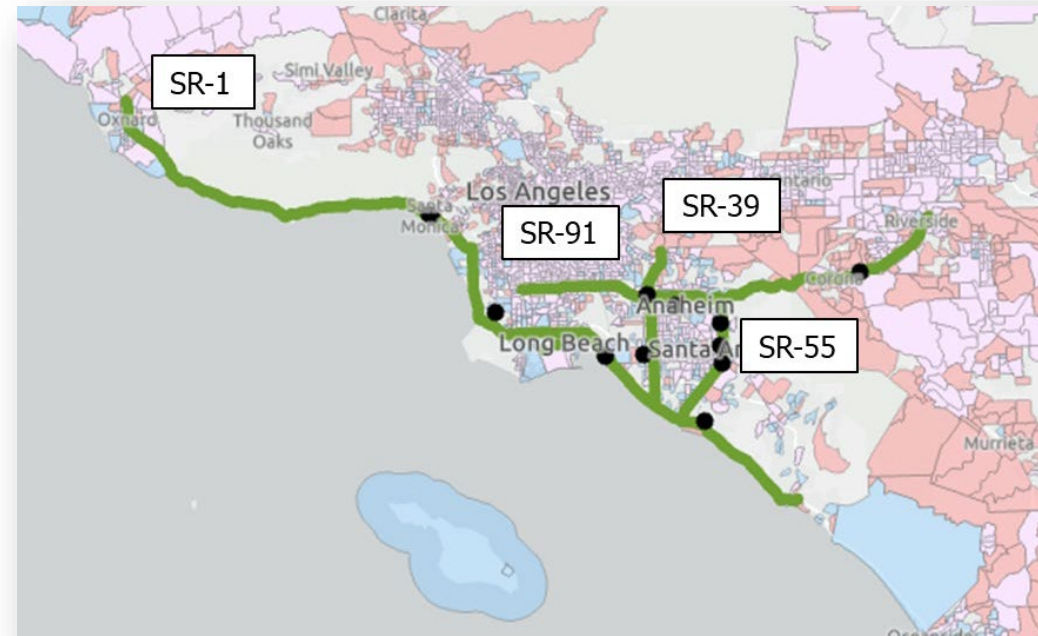
# Application Team Requirements

- Applicant must be a private entity → local governments must partner to participate
- Project team must include “experienced” Charging Network Provider
  - CEC and Caltrans define an “experienced” Charging Network Provider as a company/organization with a proven track record overseeing the procurement, installation, and maintenance of at least 20 DCFC chargers at three or more different locations and for three or more different customers in California since January 1<sup>st</sup>, 2018.

Take action early!  
Reach out to previous partners  
[EmpowerInnovation.net](https://EmpowerInnovation.net)

# Application and Awards

- Solicitation will be for multiple groups
- One application per corridor group
- Must include proposals for each segment
- CEC and Caltrans will screen and score applications
- Highest overall ranking and at least the minimum passing scoring for each corridor group will be recommended for funding
- One applicant may not receive awards for more than 3 corridor groups per solicitation



# Proposed Maximum Awards: \$146 million

## LA Region: \$10.5 million

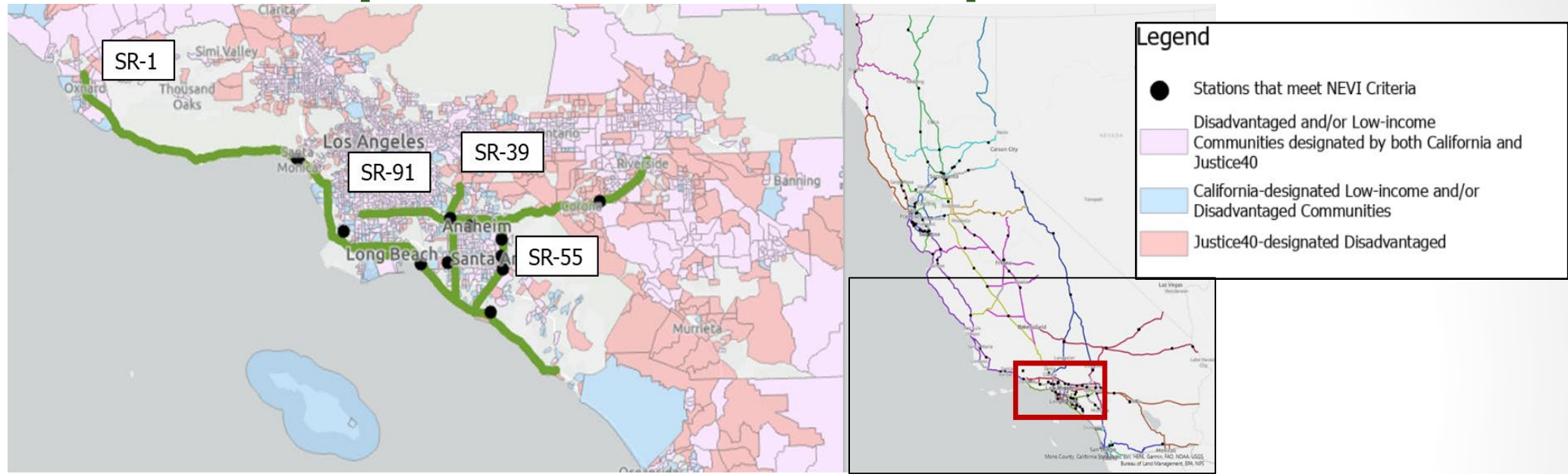
Group	Max. Award	Match Requirement
17 (LAC)	\$2.5M	50%
18 (LAC)	\$2.5M	50%
19 (LAC)	\$2.5M	50%
20 (LAC)	\$3M	50%

Maximum total cost = \$250,000 per charger

Maximum award = (Max total project cost) x (1-match requirement)

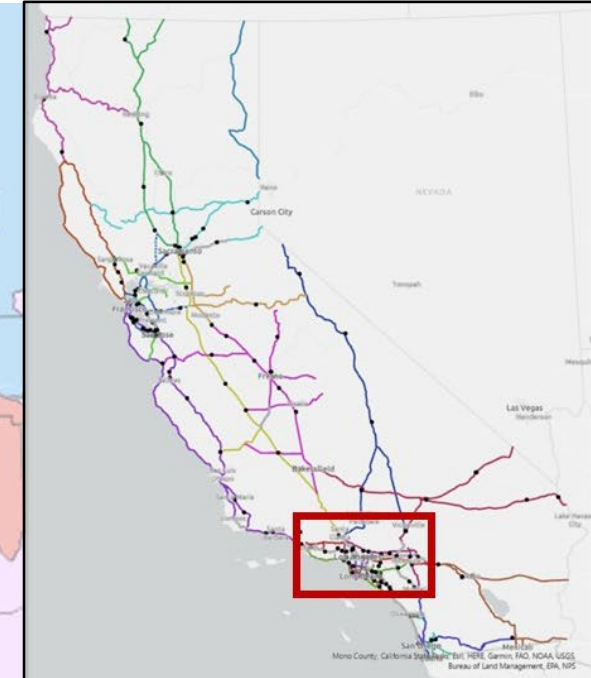
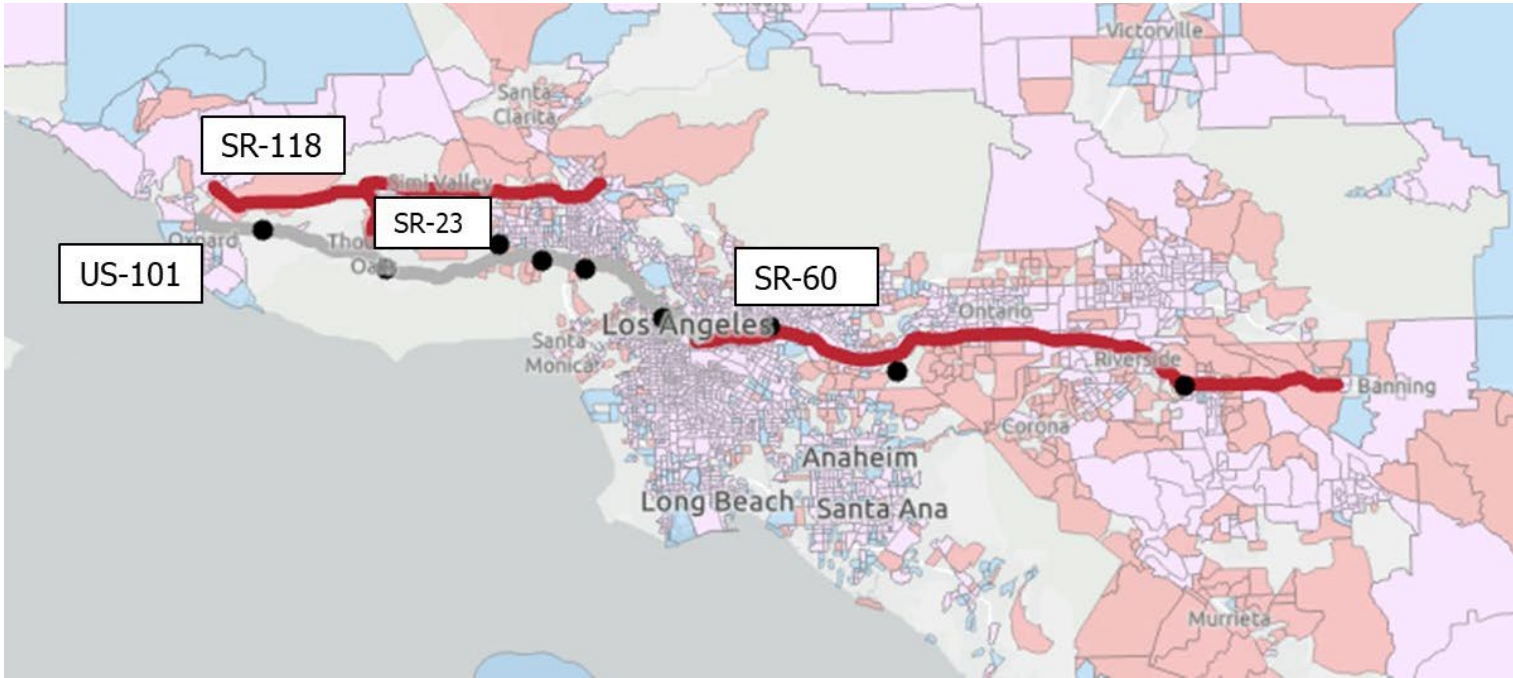


# Proposed Corridor Group #17



Group Corridor Segments	Minimum # of New Charging Stations	Number of New Chargers	Required Sites
SR39: La Habra to Huntington Beach	2	8	SR39/SR1 (La Habra), SR39/SR72 (Huntington Beach)
SR55: Anaheim to Newport Beach	1	8	SR55/SR1 (Newport Beach)
SR91: Gardena to Riverside	1	4	SR91/I-110 (Gardena)
SR1: Oxnard to Dana Point	1	4	SR1/I-5 (Dana Point)
<b>Total:</b>	<b>5</b>	<b>20</b>	

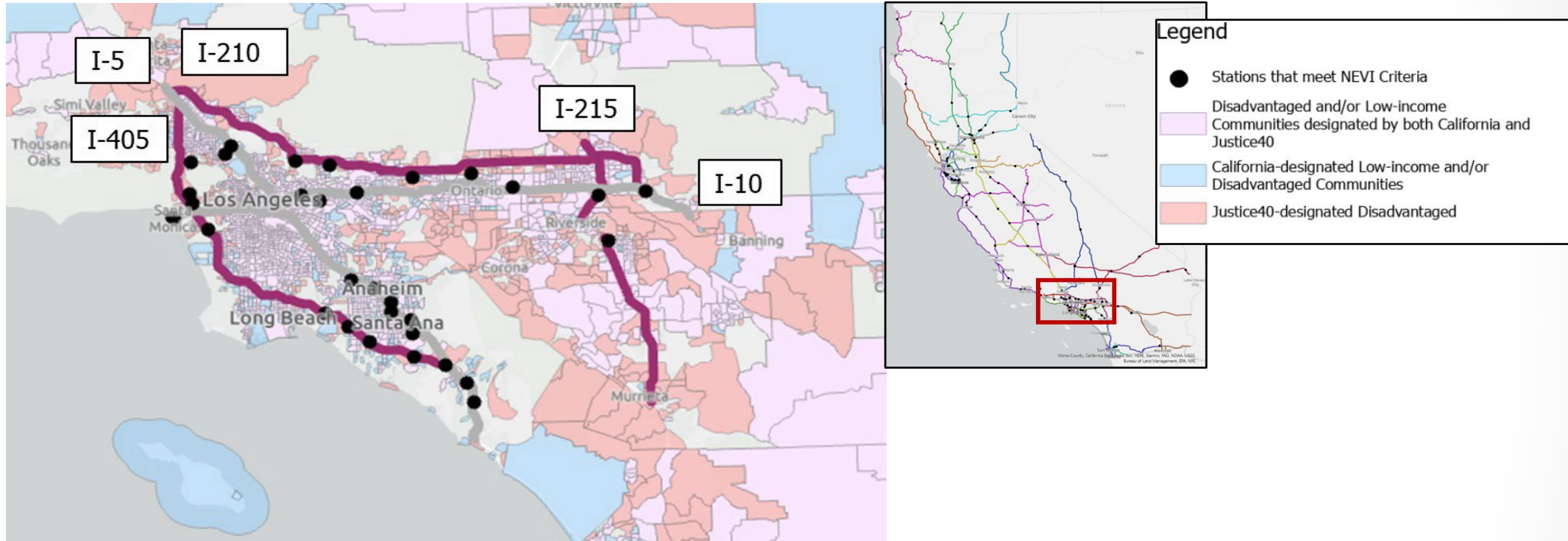
# Proposed Corridor Group #18



Group Corridor Segments	Minimum # of New Charging Stations	Number of New Chargers	Required Sites
SR23: Moonpark to Thousand Oaks	2	8	SR23/SR118 (Moonpark), SR23/US101 (Thousand Oaks)
SR118: Saticoy to San Fernando	2	8	SR118/SR126 (Saticoy), SR118/I-210 (San Fernando)
SR60: Los Angeles to Beaumont	1	4	SR60/I-10 (Los Angeles)
<b>Total:</b>	<b>5</b>	<b>20</b>	

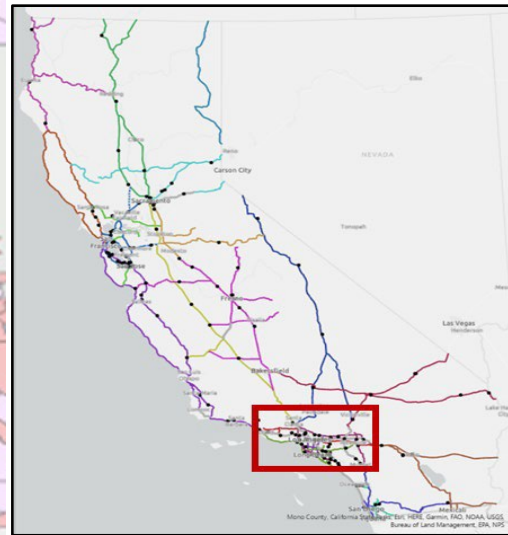
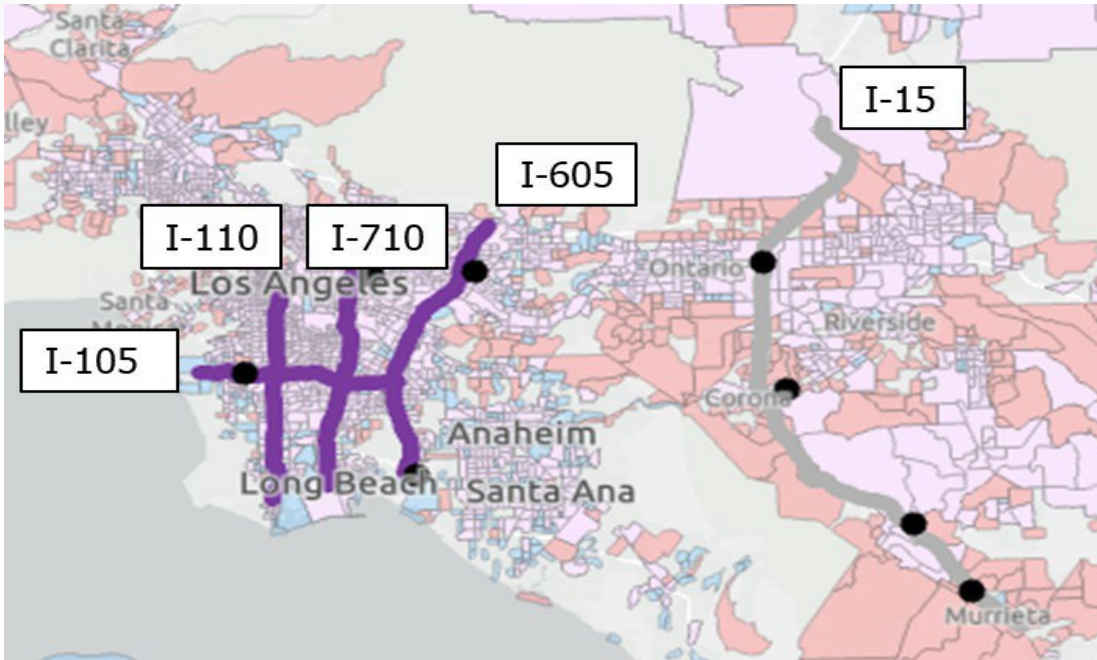


# Proposed Corridor Group #19



Group Corridor Segments	Minimum # of New Charging Stations	Number of New Chargers	Required Sites
I-210: Sylmar to Redlands	2	8	I-210/I-5 (Sylmar), I-210/I-10 (Redlands)
I-215: Murrieta to San Bernardino	2	8	I-215/I-15 (Murrieta) , I-215/I-15 (San Bernardino)
I-405: Mission Hills to Irvine	1	4	I-405/I-5 (Mission Hills)
<b>Total:</b>	<b>5</b>	<b>20</b>	

# Proposed Corridor Group #20



**Legend**

- Stations that meet NEVI Criteria
- Disadvantaged and/or Low-income Communities designated by both California and Justice40
- California-designated Low-income and/or Disadvantaged Communities
- Justice40-designated Disadvantaged

Group Corridor Segments	Minimum # of New Charging Stations	Number of New Chargers	Required Sites
I-110: Los Angeles to San Pedro	2	8	I-110/I-10 (Los Angeles), I-110/SR47 (San Pedro)
I-710: Los Angeles to Long Beach	2	8	I-710/I-10 (Los Angeles) I-710/SR1 (Long Beach)
I-605: Irwindale/Duarte to Seal Beach	1	4	I-605/I-210 (Irwindale/ Duarte)
I-105: El Segundo to Norwalk	1	4	I-105/I-605 (Norwalk)
<b>Total:</b>	<b>6</b>	<b>24</b>	

# Opportunities for Cities & Agencies

- ✓ Corridor groups spread across multiple counties provide opportunities for cities and agencies across county lines to participate
- ✓ Look to participate through partnerships
- ✓ Advocate California request discretionary funding
- ✓ Identify priority sites along AFCs
- ✓ Identify publicly owned land along AFCs (park and rides, etc.)
- ✓ Determine power level at potential sites



# Discretionary Funding

## Discretionary NEVI Funds

- Discretionary funding must be requested by California\*
- Advocating for discretionary funding open to public comment

## Charging and Refueling Federal Grants

- Local governments and community benefit organizations will have the opportunity to apply for \$2.5B in discretionary funding
- Administered at the federal level
- More information anticipated in October 2022

# Thank You!

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